



Transportation Workshop

January 28, 2016

Presented By: Diana Langley
Darin Gale
Ben Moody

Workshop Agenda



- Workshop 1 – January 28
 - Background
 - Financial Overview
 - Pavement Condition Assessment
 - Legislative Update
 - Highway 20 Improvements
 - Highway 99 Improvements
 - Bridge Street
- Workshop 2 – February 23
 - Garden Highway Industrial Area
 - Caltrans ATP Grants
 - Citywide Traffic Model Update
 - Bicycle Master Plan Implementation
 - Local Transportation Projects
 - Discussion of Funding Options

PURPOSE

Purpose of Workshop



- Provide high level overview of various projects
- Establish priority for projects
- Discuss options to address maintenance needs



BACKGROUND

Background



- Several workshops held since 2013
- Priorities:
 - Fifth Street Bridge
 - Bridge Street – SR 99 to Fifth Street Bridge
 - Road Rehabilitation/Maintenance
 - SR 20 Revitalization
 - SR 99 Widening – SR 20 to Bogue Road
 - Lincoln Road Extension east of Garden Highway



FINANCIAL OVERVIEW

Sources of Funds



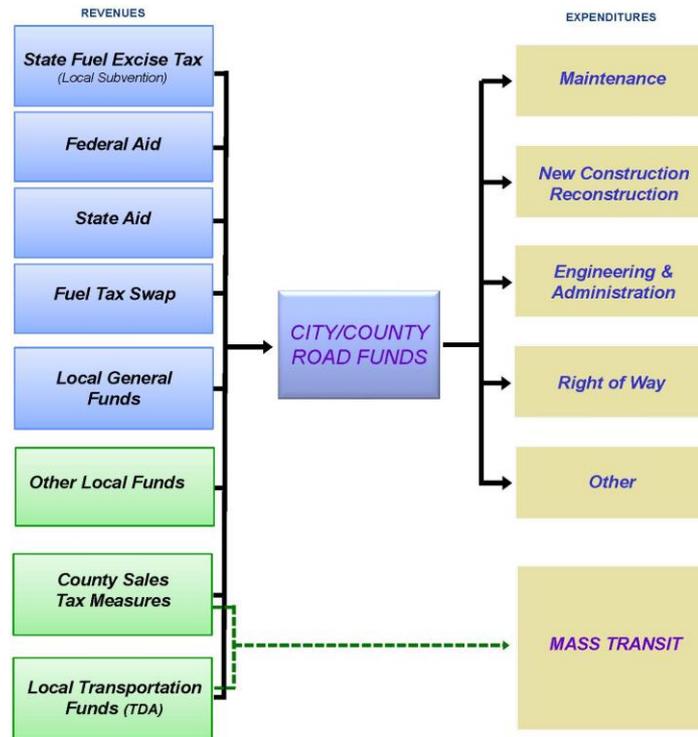
- Road Fund:
 - Local Transportation Fund
 - Gas Tax
 - Highway Users Tax (Motor Vehicle Fuel Tax)
 - Road Maintenance Franchise Fee
 - Investment Income
- Developer Impact Fees – Roads
- Grants

Sources of Funds



Chart 9

Local Street and Road Funding



Road Fund Overview



| | Actual FY 11/12 | Actual FY 12/13 | Actual FY 13/14 | Actual FY 14/15 | Projected FY 15/16 |
|-----------------------------|--------------------|--------------------|--------------------|--------------------|-----------------------|
| Revenues: | | | | | |
| LTF | \$1,345,087 | \$1,464,744 | \$1,696,190 | \$1,289,439 | \$1,540,935 |
| Gas Tax | \$934,943 | \$955,521 | \$1,130,893 | \$1,036,708 | \$1,167,974 |
| HUTA | \$881,719 | \$515,127 | \$908,992 | \$609,503 | \$309,474 |
| Franchise Fee | \$24,547 | \$88,139 | \$105,726 | \$188,633 | \$220,000 |
| Interest & Other Revenue | \$340,276 | \$66,976 | \$89,038 | \$401,737 | \$17,100 |
| Grants | \$323,351 | \$1,497,616 | \$1,381,566 | \$1,220,105 | \$5,168,287 |
| Total: | \$3,849,923 | \$4,588,123 | \$5,312,405 | \$4,746,125 | \$8,423,770 |
| Expenditures: | | | | | |
| Operational Costs: | \$1,789,201 | \$1,483,245 | \$1,599,650 | \$1,504,050 | \$1,583,900 |
| CIP: | \$1,874,024 | \$2,700,813 | \$3,052,128 | \$2,540,220 | \$7,554,899 |
| Total: | \$3,663,225 | \$4,184,058 | \$4,651,778 | \$4,044,270 | \$9,138,799 |

Road Fund Overview



| | Actual FY 11/12 | Actual FY 12/13 | Actual FY 13/14 | Actual FY 14/15 | Projected FY 15/16 |
|--------------------------------------|--------------------|--------------------|--------------------|--------------------|-----------------------|
| Revenues Without Grant Funds: | | | | | |
| Total: | \$3,526,572 | \$3,090,507 | \$3,930,839 | \$3,526,202 | \$3,255,483 |
| Expenditures: | | | | | |
| Operational Costs: | \$1,789,201 | \$1,483,245 | \$1,599,650 | \$1,504,050 | \$1,583,900 |
| \$ Available for CIP | \$1,737,371 | \$1,607,262 | \$2,331,189 | \$2,021,970 | \$1,671,583 |

Road Fund Overview



| | Projected FY 15/16 | Projected FY 16/17 | Projected FY 17/18 | Projected FY 18/19 | Projected FY 19/20 |
|--------------------------------------|-----------------------|-----------------------|-----------------------|-----------------------|-----------------------|
| Revenues Without Grant Funds: | | | | | |
| Total: | \$3,255,483 | \$3,070,000 | \$3,200,000 | \$3,300,000 | \$3,400,000 |
| Expenditures: | | | | | |
| Operational Costs: | \$1,583,900 | \$1,630,000 | \$1,680,000 | \$1,730,000 | \$1,780,000 |
| \$ Available for CIP | \$1,671,583 | \$1,440,000 | \$1,520,000 | \$1,570,000 | \$1,620,000 |

Assumption:

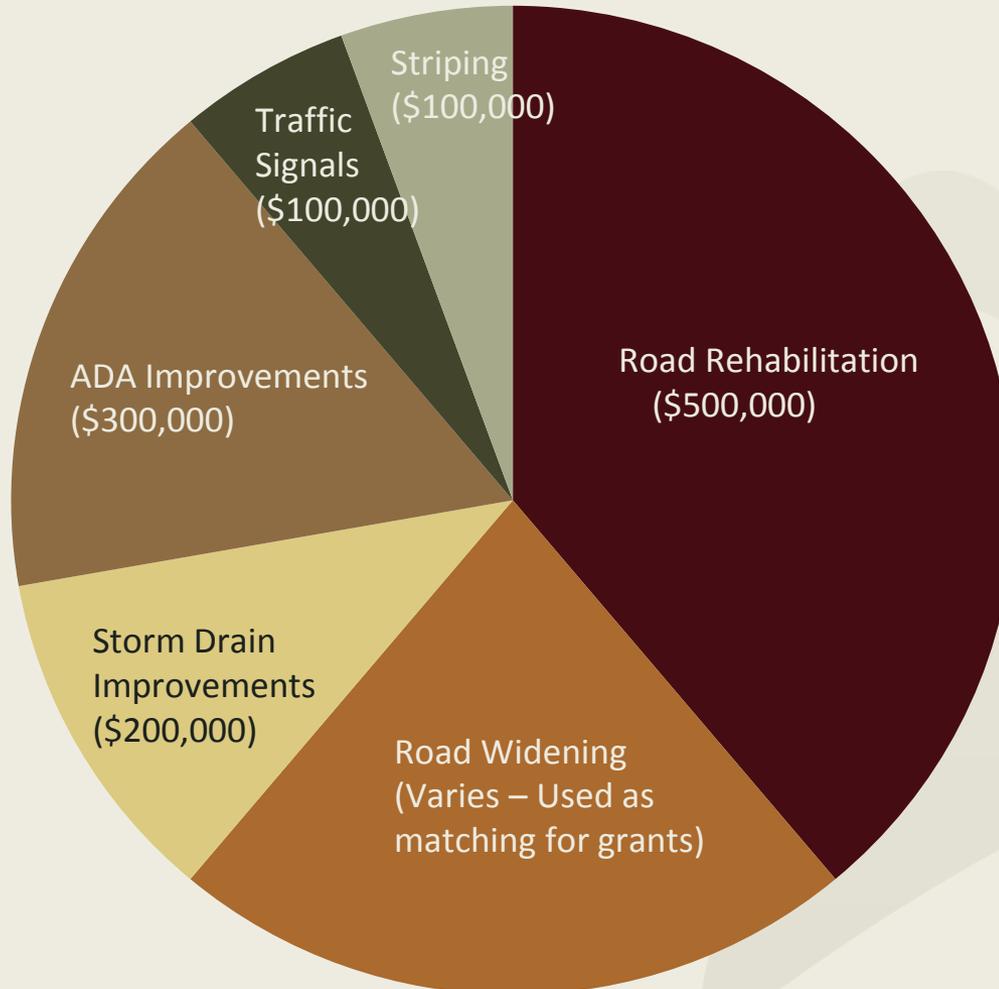
- Operational Costs increase 3% annually.

Uses of Road Funds



- Operational Costs
 - Salaries & Benefits
 - Materials, Services & Supplies
- Capital Improvement Projects
 - Road Rehabilitation
 - Road Widening/New Construction
 - Storm Drain Improvements
 - Sidewalk/ADA Improvements
 - Traffic Signals
 - Streetlights
 - Striping of Roadways

Uses of Road Funds



Developer Impact Fee - Roads Overview



| Land Use | Developer Impact Fee - Roads | Infill Developer Impact Fee - Roads |
|--------------------------|---------------------------------|--|
| Single Family Dwelling | \$10,285.72 | \$5,142.86 |
| Multi-Family Residential | \$5,958.44 | \$2,979.22 |
| Duplex | \$10,866.41 | \$5,433.21 |
| Mobile Home | \$6,270.71 | \$3,135.36 |
| Commercial | \$8.64/SF | \$4.32/SF |
| Office | \$3.11/SF | \$3.04/SF |
| Industrial | \$0.53/SF | \$0.53/SF |
| | | |

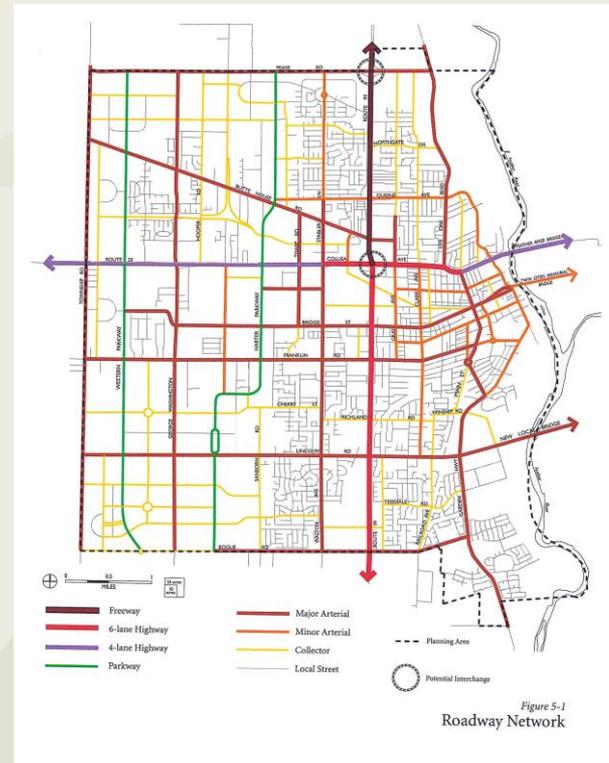
Uses of Developer Impact Fee

- Road Funds



- Major Planned Roadway Improvements Identified in the General Plan

| | |
|---|----------------------|
| Estimated Cost of Major Planned Roadway Improvements | \$336,000,000 |
| DIF – Road Funds Available as of 12/31/15 | \$4,758,000 |





PAVEMENT CONDITION ASSESSMENT

BEN MOODY

CITY ROAD STATUS



- Maintained Centerline Miles of Roadways
 - 227 Miles within the City Limits
 - Arterials - 30 Miles
 - Collectors - 34 Miles
 - Local Roads – 163 Miles

- Pavement Assessment

- **Current Yuba City PCI – 67**

- Current Goal PCI > 70
 - Based on report generated in 2015
 - Pavement Management System Developed through FY 15/16 CIP “StreetSaver” with NCE consulting performing assessment
 - Recommended to update the assessment 3 – 5 years
 - » Determine maintenance needs
 - » Develop strategy for maintenance

| | | |
|--------------------|---|-----|
| | | 100 |
| I | Good | 70 |
| II/III | <div style="background-color: blue; color: white; text-align: center; padding: 2px;">Fair (non-load)</div> <div style="background-color: yellow; text-align: center; padding: 2px;">Fair (load-related)</div> | 50 |
| IV | Poor | 25 |
| V | Very Poor/Failed | 0 |
| Condition Category | Pavement Condition | PCI |

Pavement Condition Categories

PAVEMENT CONDITIONS



- Yuba City PCI 67, Goal is > 70

QUEENS AVE., (GOOD) – PCI 81



GARDEN HWY, (POOR) – PCI 39



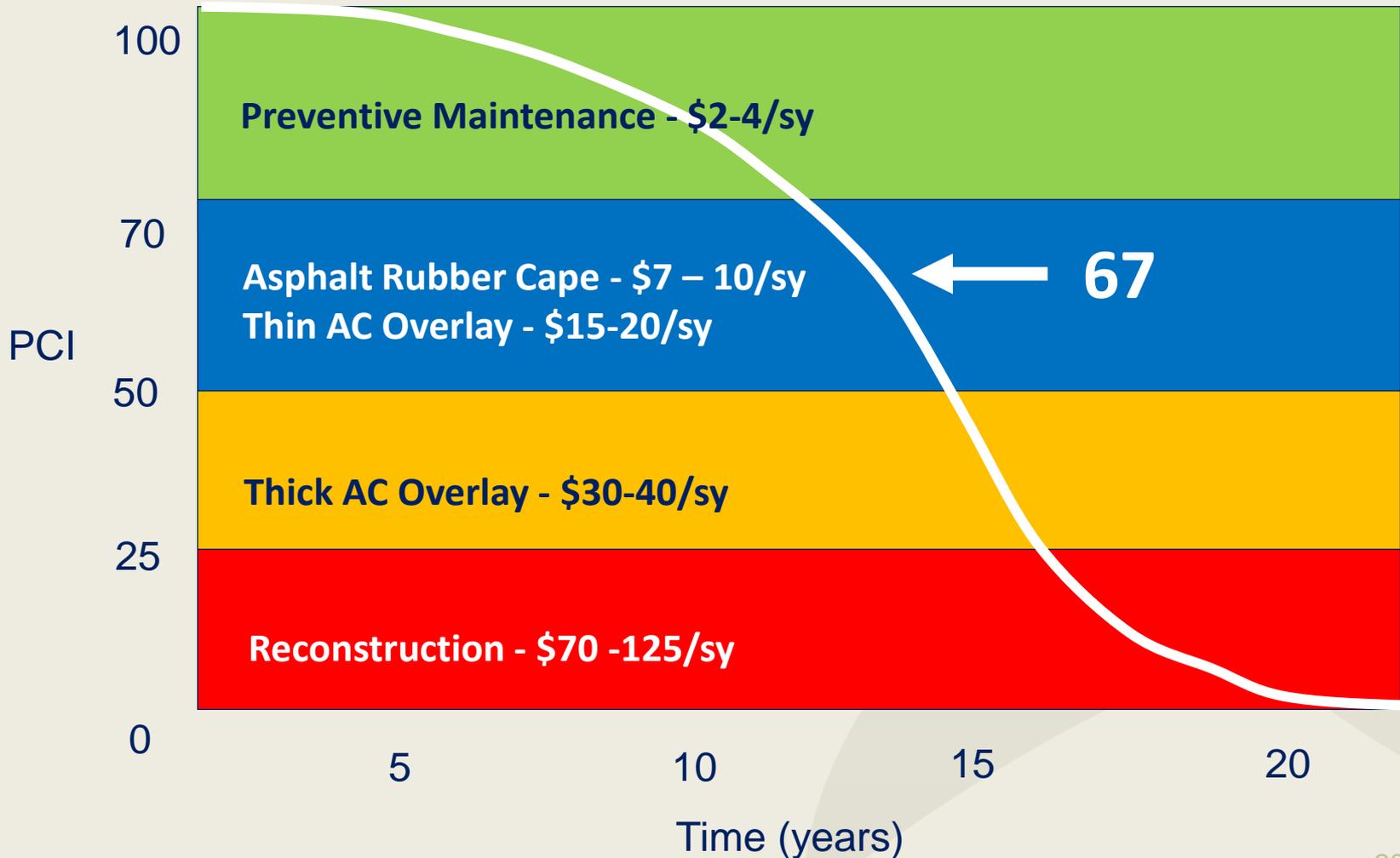
WALTON AVE., (FAIR) - PCI 69



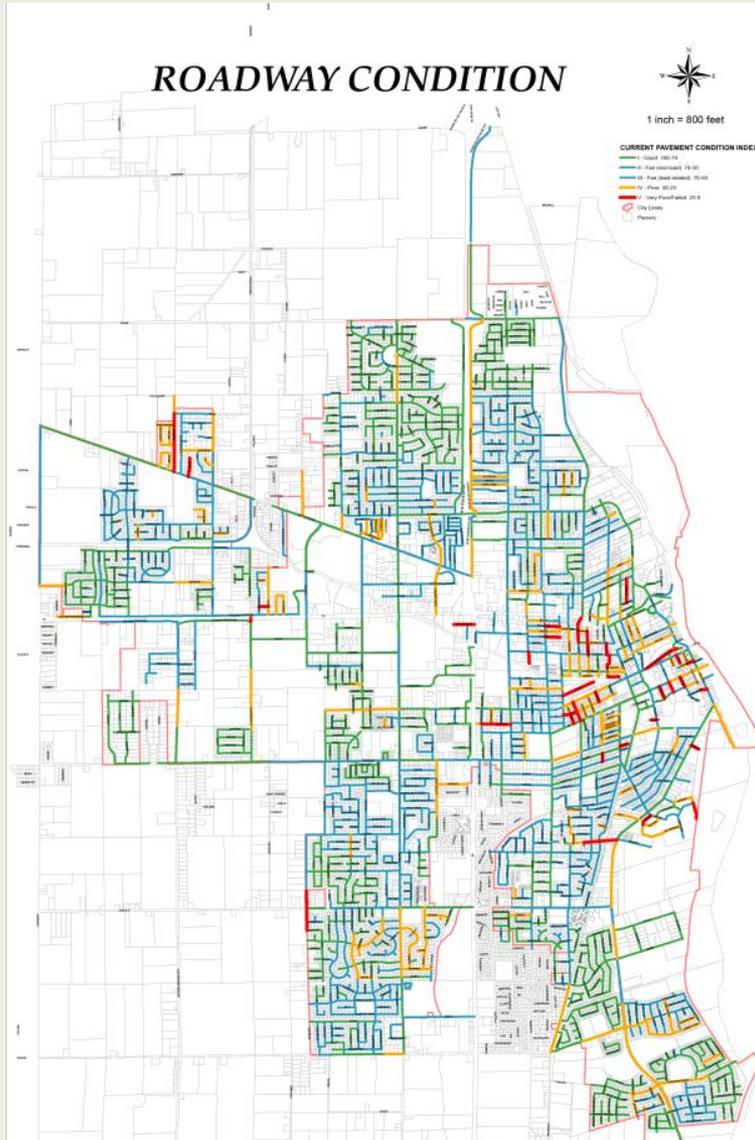
SANBORN RD., (FAILED) – PCI 23



Why is PCI so critical?



ROAD CONDITIONS



CITY ROAD STATUS



Cost Scenarios:

- Scenario 1: City funding **\$0.5M/yr**
PCI will drop to 34 by 2035
- Scenario 2: City funding **\$1.5M/yr**
PCI will drop to 42 by 2035
- Scenario 3: City funding **\$5.9M/yr**
Maintain current PCI at 67
- Scenario 4: City funding **\$6.4M/yr**
PCI will improve to 70
- Scenario 5: City funding **\$8M/yr**
PCI will Improve to 85

SANBORN RD., (FAILED) – PCI 23

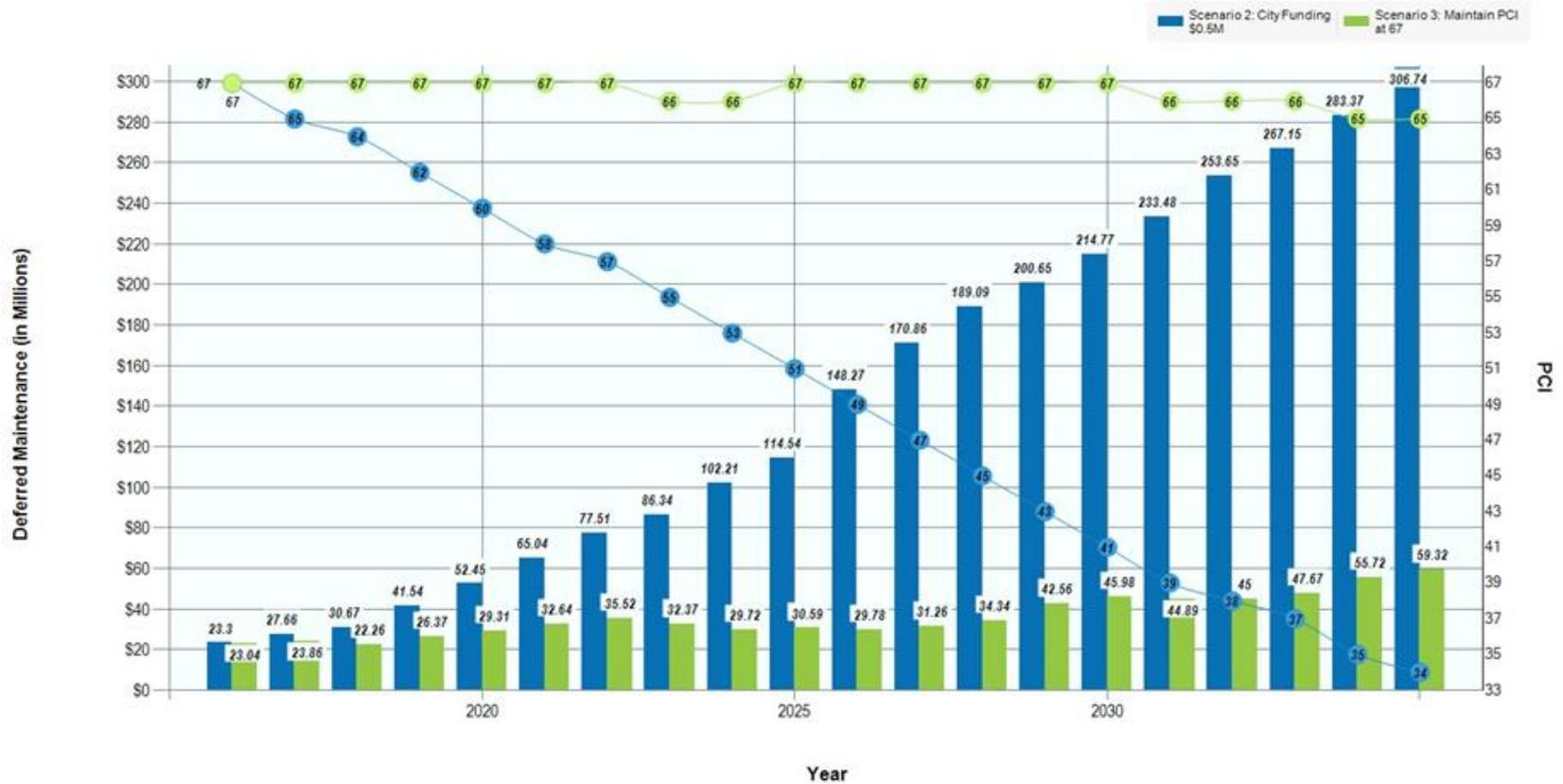


| Agency | PCI | Centerline miles | Cost to maintain current PCI(\$M/Yr) |
|-----------|-----|------------------|--------------------------------------|
| Yuba | 67 | 227 | \$5.9 |
| Folsom | 77 | 274 | \$8.4 |
| Fairfield | 72 | 320 | \$8.8 |

ROAD CONDITIONS



Scenario Comparison - Deferred Maintenance and PCI



FUNDING / PROJECTS



- Current funding levels
 - Not meeting needs, quality reduction, reactive approach
 - Grant funding has not been for maintenance or rehab. Projects
- Matching funds – existing/future projects
 - Necessary to maximize returns
- Future project selection
 - Determination of approach/priorities
 - Arterials / Collectors / Local Roads



LEGISLATIVE UPDATE DARIN GALE

Legislative Update



- Fix Our Roads Coalition
- Republican Caucus Proposal
- Will the State or Federal government solve the problem?

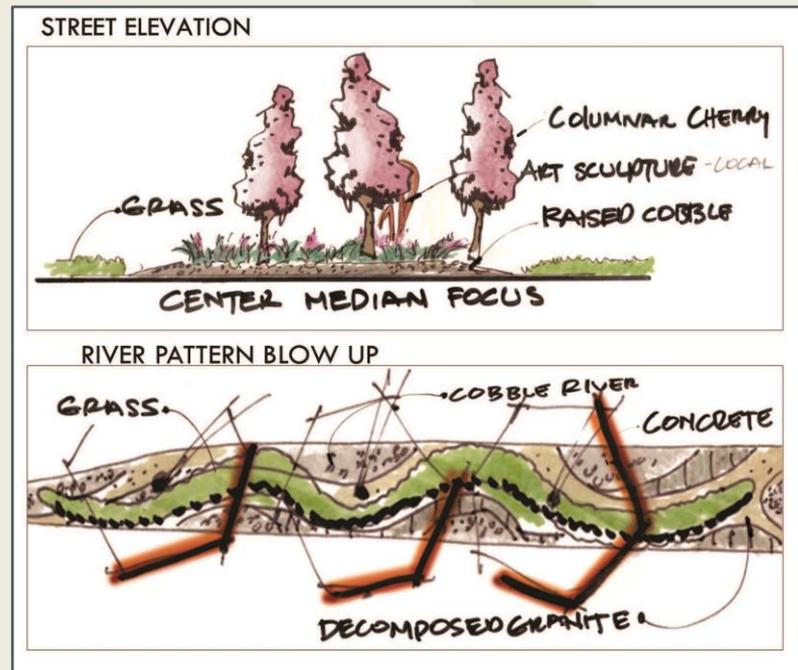


HIGHWAY 20 IMPROVEMENTS

Highway 20 Improvements



- SR 20 Revitalization Project
 - Phase 1 – Stafford Way to Clark Avenue
 - Phase 2 – Preliminary Design – SR 99 to 10th Street Bridge including SR 20/SR 99 gateway enhancements





HIGHWAY 99 IMPROVEMENTS

Highway 99 Improvements



- Operational Improvements – SR 20 to Bogue Road
- Future Interchange at SR 99/SR 20
- Future Interchange at SR 99/Pease Road

SR 99 Operational Improvements



- Caltrans Project
- Need:
 - Peak period traffic condition monitoring shows steady increases in both the duration and length of congestion on the Yuba City SR 99 corridor
- Purpose:
 - Improve safety
 - Efficiently manage traffic operations
 - Reduce congestion
 - Decrease peak hours of delay

SR 99 Operational Improvements



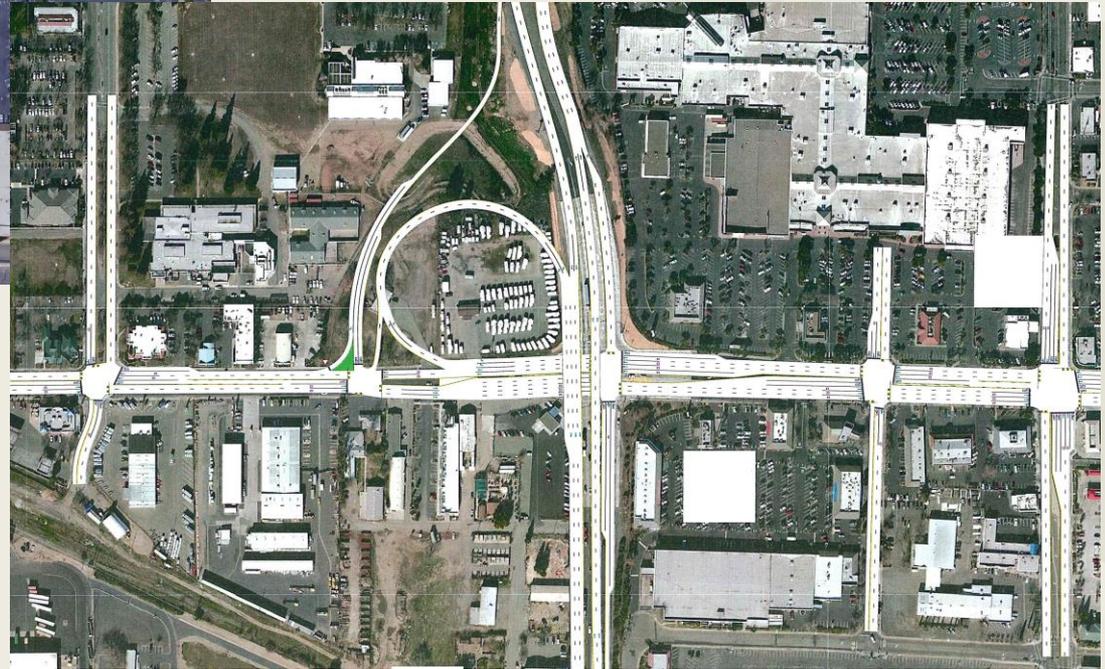
- Scope of Work:
 - Install traffic signal at Hunn Road
 - Extend right-turn lanes
 - Northbound SR 99 to eastbound SR 20
 - Northbound SR 99 to eastbound Bridge Street
 - Install overhead changeable message signs
 - Southbound near Pease Road
 - Northbound between Franklin Road and Bridge Street
 - Upgrade communication systems, vehicle sensors, curb ramps and pedestrian sensors and signals between Lincoln Road and SR 20

SR 99 Operational Improvements



- Cost Estimate - \$2,453,900
- Schedule
 - Right-of-Way Certification 2/1/16
 - Fund Allocation 4/1/16
 - Award of Contract 8/1/16
 - Project Complete 10/1/17

Future Interchange at SR 99/SR 20

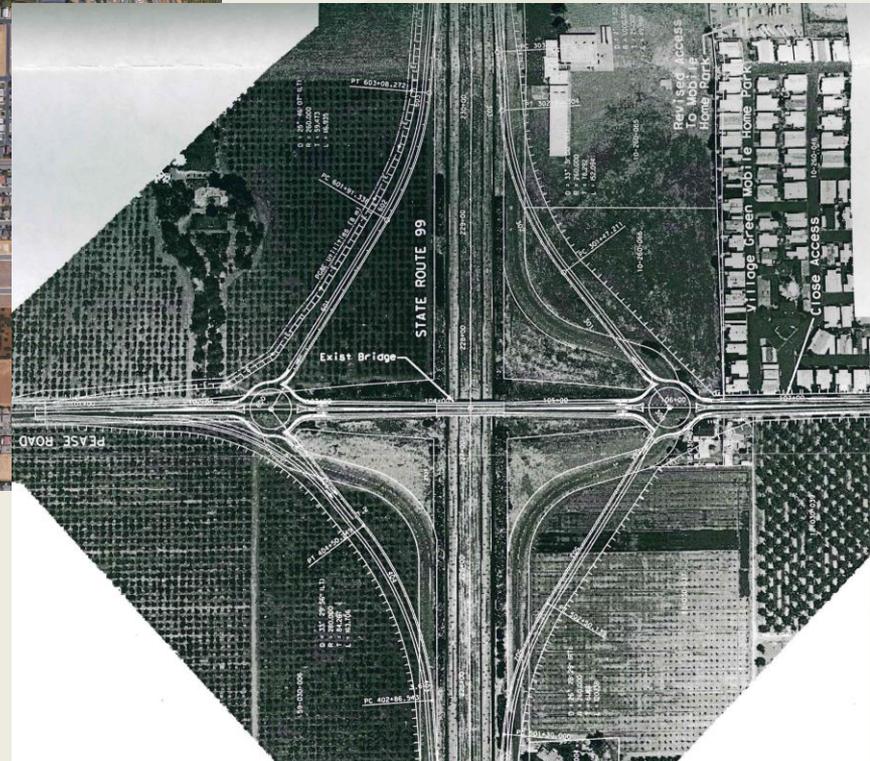


Future Interchange at SR 99/SR 20



- Caltrans requires a Project Study Report – Project Development Support (PSR-PDS) Project Initiation Document to analyze interchange alternatives
- Caltrans has provided a proposal to prepare the PSR-PDS for up to 4 interchange alternatives
 - Estimate 1 - \$292,000
 - Estimate 2 - \$437,000 (Estimate 1 scope of work + traffic modeling)

Future Interchange at SR 99/Pease Road



Future Interchange at SR 99/Pease Road



- Project Study Report (PSR) completed in December 2004
- Evaluated 3 Interchange Alternatives:

| Alternative | Description | Estimated Cost* |
|-------------|--|-------------------|
| 1 | Tight Diamond Interchange with Roundabouts | \$12-\$15 Million |
| 2 | Partial Cloverleaf Interchange (L-9) | \$15-\$20 Million |
| 3 | Partial Cloverleaf Interchange (L-7) | \$18-\$22 Million |

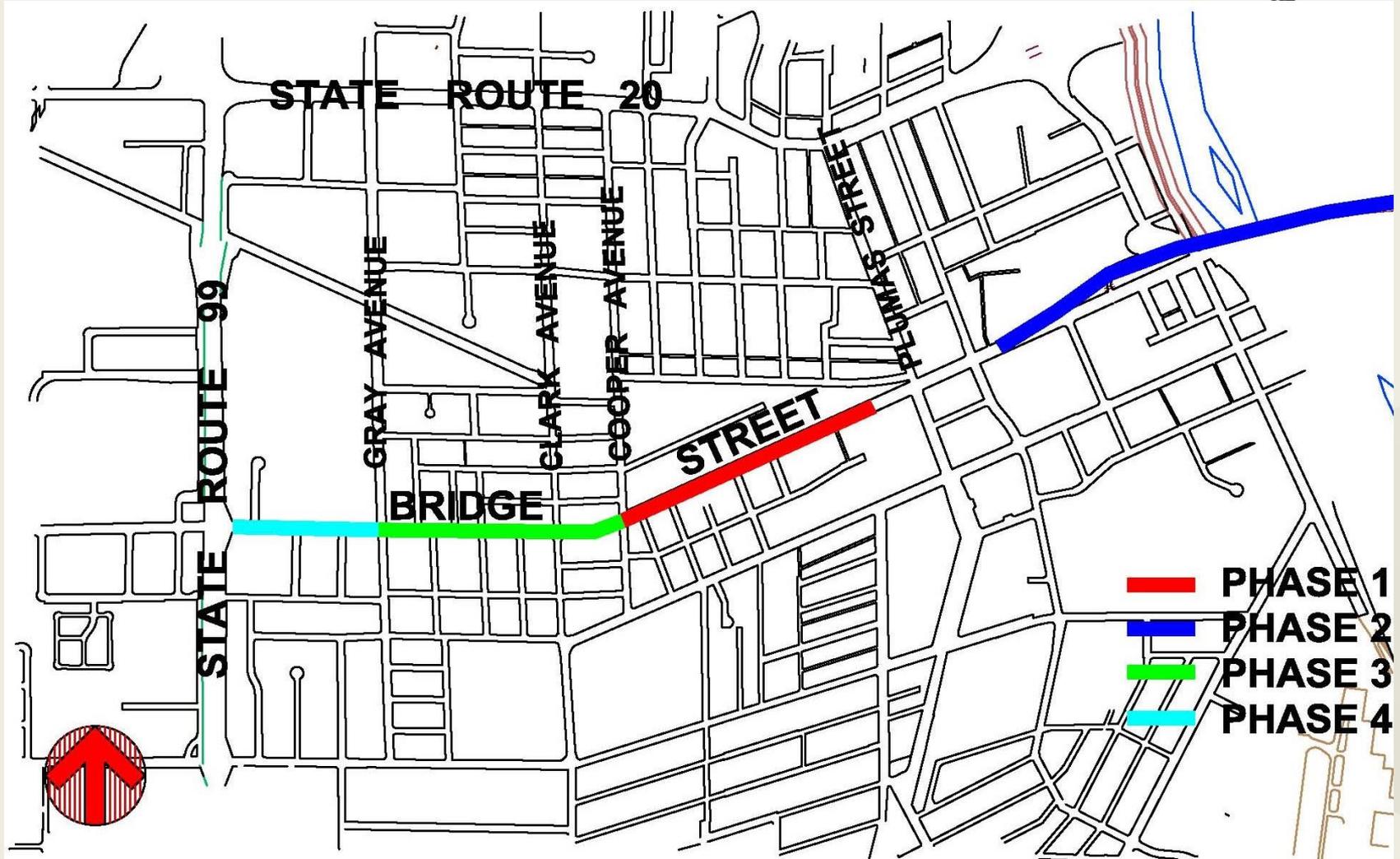
*Estimated cost would be considerably higher today.

If this is determined to be a priority project, then the PSR would need to be updated.



BRIDGE STREET

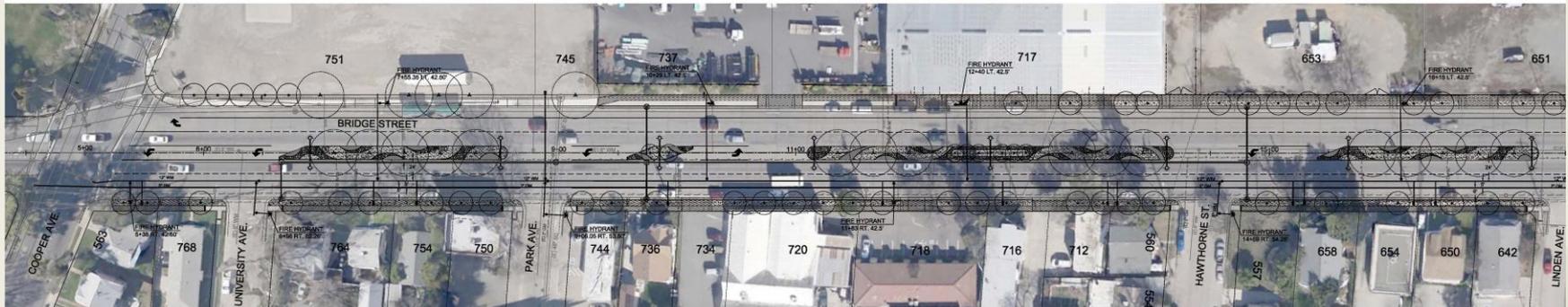
Bridge Street - Phasing



Bridge Street – Phase 1



Cooper Avenue to Plumas Street



Bridge Street – Phase 2

5th Street Bridge



5th Street Bridge

Next Steps



- Execution of High Cost Project Agreement – Fall 2016
- Design Complete – November 2016
- Right-of-Way Certified – December 2016
- Construction
 - 2 Years to Complete
 - Currently anticipated to start July 2017 with completion in October 2019
 - Advance work will begin this Fall

5th Street Bridge Funding



- Project Estimate - \$70 Million
- Breakdown of Funding:

| Funding Source | Amount |
|--|--------------|
| Highway Bridge Program | \$46,400,000 |
| Regional Surface Transportation Program | \$3,200,000 |
| Regional Improvement Program/State Transportation Improvement Program | \$17,400,000 |
| Local Funds | \$2,600,000 |
| Total: | \$69,600,000 |

*The PSR/PR phase was funded through the High Priority Projects program.

Bridge Street – Phase 3

Gray Ave. to Cooper Ave.



Three Options for Consideration

- Option A-1 – Provide a frontage road along the south side of Bridge Street
- Option A-2 – Build section similar to Phase 1 project with landscape median and planters
- Option A-3 – Build within existing right-of-way

Bridge Street – Phase 3 Gray Ave. to Cooper Ave.



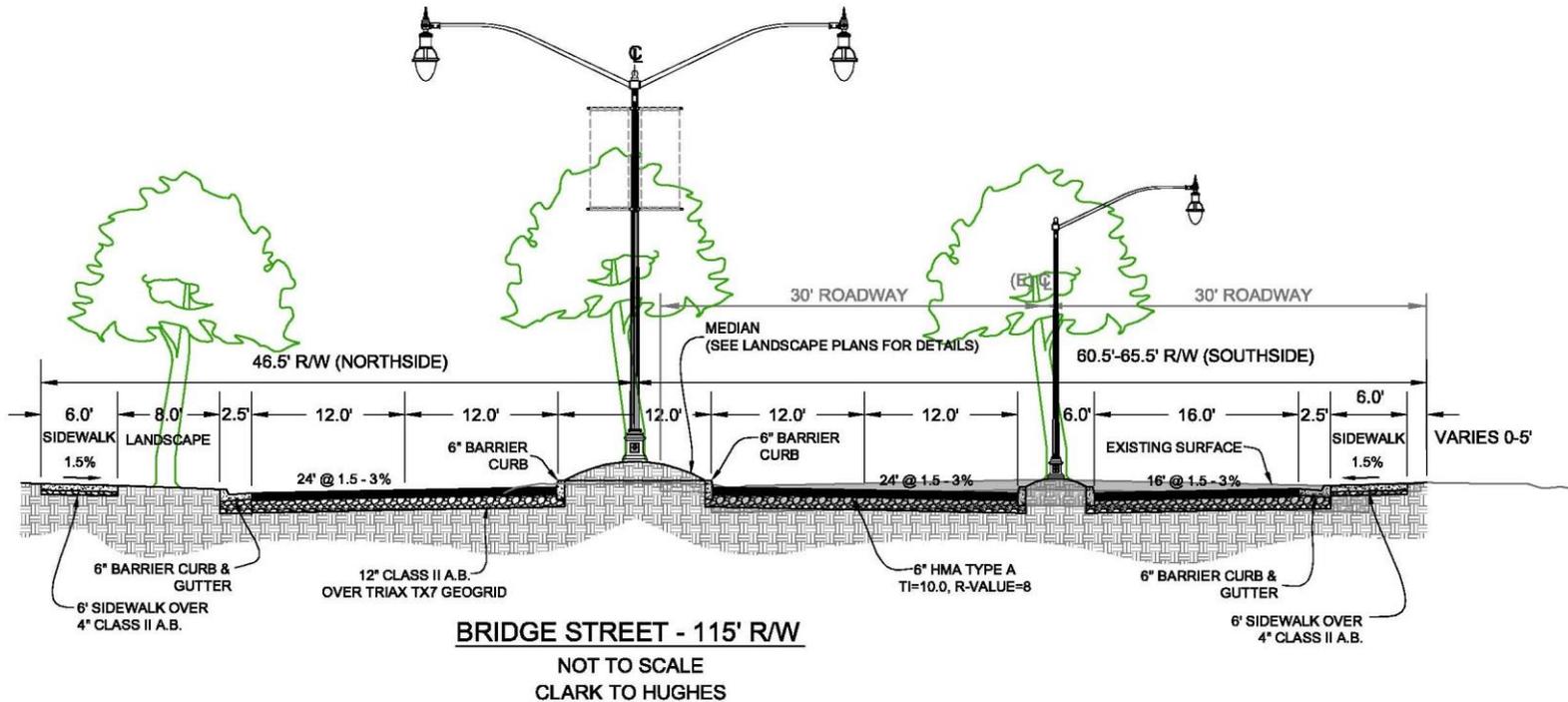
Three Options for Consideration

| Option | Description | Right-of-Way Required | Estimated Cost |
|--------|--|-----------------------|----------------|
| A-1 | Standard Travel Lanes, Landscape Median, Frontage Road on South Side | Yes | \$8,400,000 |
| A-2 | Standard Travel Lanes, Landscape Median, Parking on South Side | Yes | \$7,500,000 |
| A-3 | 5 Narrow Travel Lanes, No Parking | No | \$2,300,000 |

Bridge Street – Phase 3 Option A-1



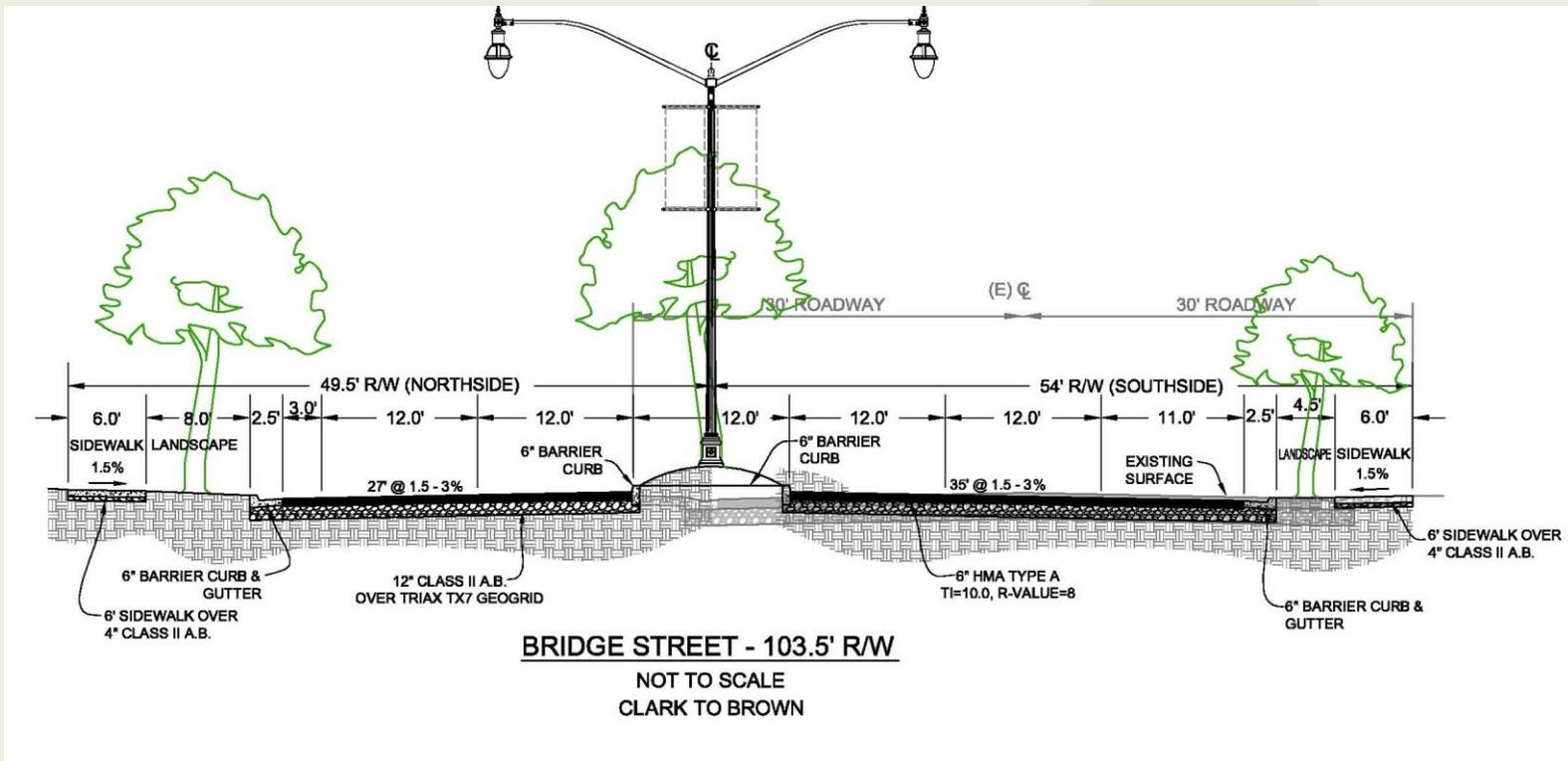
| Option | Description | Right-of-Way Required | Estimated Cost |
|--------|--|-----------------------|----------------|
| 1 | Standard Travel Lanes, Landscape Median, Frontage Road on South Side | Yes | \$8,400,000 |



Bridge Street – Phase 3 Option A-2



| Option | Description | Right-of-Way Required | Estimated Cost |
|--------|--|-----------------------|----------------|
| 2 | Standard Travel Lanes, Landscape Median, Parking on South Side | Yes | \$7,500,000 |

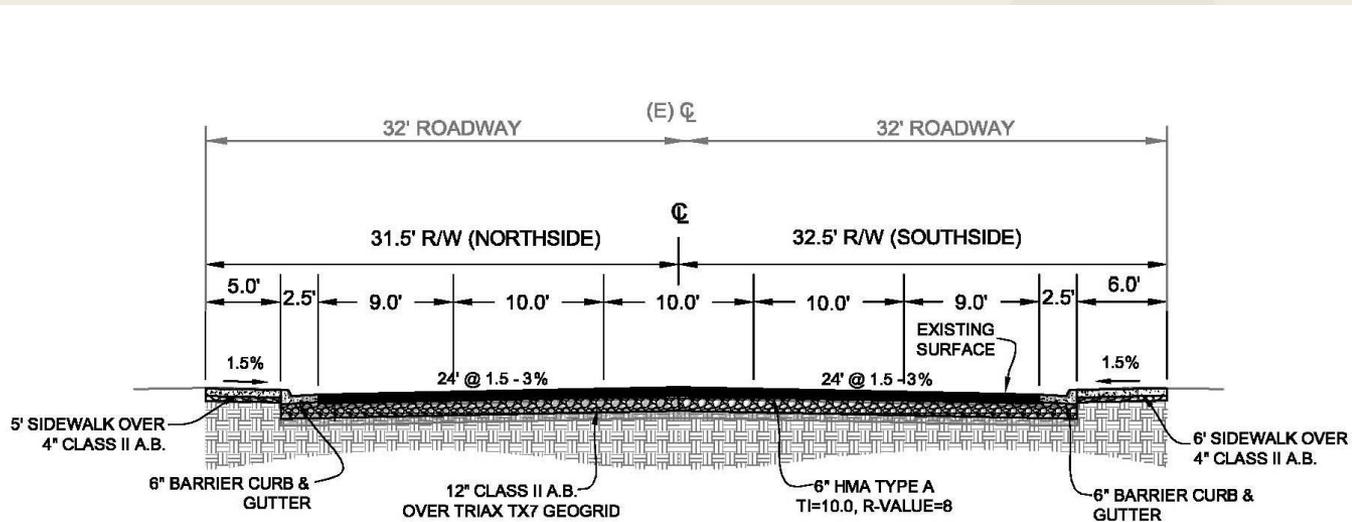


Bridge Street – Phase 3

Option A-3



| Option | Description | Right-of-Way Required | Estimated Cost |
|--------|-----------------------------------|-----------------------|----------------|
| 3 | 5 Narrow Travel Lanes, No Parking | No | \$2,300,000 |



BRIDGE STREET - 64' R/W

NOT TO SCALE
COOPER TO CLARK

Bridge Street – Phase 4 SR 99 to Gray Avenue



Bridge Street – Timing



| Phase | Description | Anticipated Completion Date |
|-------|--------------------------------|---------------------------------------|
| 1 | Cooper Avenue to Plumas Street | Summer 2016 |
| 2 | Fifth Street Bridge | October 2019 |
| 3 | Gray Avenue to Cooper Avenue | Needs to be completed by October 2019 |
| 4 | SR 99 to Gray Avenue | TBD |

WRAP UP

Wrap-Up



Developer Impact Fees

| | |
|---|----------------------|
| Estimated Cost of Major Planned Roadway Improvements | \$336 Million |
| DIF – Road Funds Available as of 12/31/15 | \$5 Million |

Road Funds

| | |
|---|----------------------|
| Estimated Cost of Funds Required to Maintain Current PCI | \$5.9 Million |
| Road Funds Currently Available | \$0.5 Million |

Workshop 2 Agenda



- Workshop 2 – February 23
 - Garden Highway Industrial Area
 - Caltrans ATP Grants
 - Citywide Traffic Model Update
 - Bicycle Master Plan Implementation
 - Local Transportation Projects
 - Discussion of Funding Options